

Development Services Staff Report for Design Review Committee

To: Design Review Committee
From: DS Staff: Erika Akin
Subject: Proposed Addition to J's Car Wash
File: DSRFY2016-1
Date: January 19, 2016

Summary: *J's Car Wash*, represented by Josh Howa of Howa Design, is proposing to expand the current vehicle washing facility at 3756 West Chinden. The proposal requires Design Review Committee approval to add a traffic/stacking lane onsite, modify existing landscaping, add a vehicle access to Chinden Blvd., remove fencing, add on to the existing building and redesign the site. The applicant is proposing the addition of landscape areas to adjacent properties through the removal of asphalt along the existing Chinden sidewalks. The DR application includes site design, landscaping plan, building elevations and schematics. The project is zoned C-1 General Commercial and Mixed Use Commercial and Green Boulevard Corridor Comprehensive Plan designation.

Analysis:

Staff has completed a preliminary review of the proposed use and plans. The following are items for discussion and while some of them may not relate directly to the Committee's purview, they may present challenges for implementing ordinances pertaining to design review:

- Adding a Chinden Boulevard access violates Code; Consider escape route to Osage instead of Chinden
- Tree mitigation for removal of five existing trees onsite not provided.
- Stacking lane may not be screened as required in Drive Through provisions
- A safe pedestrian route for customers to and from their vehicle to the waiting areas has not been identified on the plans
- Existing drainage swale modification and onsite drainage retention may be difficult
- Street trees added to adjacent properties improve entire block of streetscape
- Will adjacent property owners allow landscaping beds to be reworked? Are agreements in place to allow the installation?
- Who will maintain new landscaping?
- How will the new detail parking places be accessed?
- Cross access agreements (cars exit from J's onto Jacksons property)

GC Code

8-2C-014 Drive Through Establishment:

A. Location Standards

1. A drive through establishment may be appropriate on Chinden, Glenwood or State Street, which are arterial roadways that carry high volumes of pass through traffic, however the use is not appropriate on a local or collector streets.

2. A drive-through establishment use shall not be closer than five hundred (500') feet from a residence, park; or a school; and:

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- a. The speaker system is located so that the sound from a speaker system is directed away from a residence, park, or school and not audible off the site; and
- b. Stacking does not create an impact off site; and
- c. Vibration, noise, odors, etc. is not allowed off site; and
- d. The use does not otherwise create a negative impact.

3. A drive-through establishment shall not be closer than five hundred (500'), as measured by the shortest unobstructed driving route from another drive-through establishment.

B. Building Design and Site Layout.

1. New drive-through facilities shall be oriented toward the side or rear yards and not placed between the street right-of-way and the primary customer entrance.
2. Vehicular lanes shall be sited to minimize additional vehicular traffic on site and within the surrounding uses.
3. Service shall be provided to those who elect to walk or bike.
 - a. Service shall be available in an area that is protected from the weather and separate from vehicle stacking lanes.
4. Adequate trash receptacles shall be provided that complies with the requirements set forth in section 8-4A-6 OUTDOOR SERVICE AND EQUIPMENT AREAS of this Title.
5. All site and vehicular lighting shall be down-shielded, screened or oriented not to encroach on any residence.
6. Stacking lanes shall be screened with landscaping by plantings or berming or a combination of both to a minimum height of four feet (4') where not in conflict with a necessary clear vision triangle.

C. Access and Parking. Safe pedestrian and vehicle access and circulation on the site and between adjacent properties shall be demonstrated as follows:

1. A bike rack shall be provided.
2. Access to the pedestrian customer shall be provided facing the public right-of-way.
3. Direct, comfortable, and safe pedestrian connection from the public sidewalk system and the parking area to the structure shall be provided through sidewalks or pathways that are:
 1. A minimum of 5' in width; and
 2. American with Disability Act compliant; and
 3. Uninterrupted by motorized vehicles. If there is no other feasible alternative, the vehicular crossing of the path or sidewalk shall be designed in a way to visibly demonstrate that it is secondary to the pedestrian. This may be achieved changes in materials or colors, landscaping, or other such treatments.
- A. Based on the size of the drive-through and expected patronage, stacking lanes shall have sufficient capacity to prevent obstruction of the public right-of-way.
- B. The stacking lane shall be a separate lane from the circulation lanes needed for access and parking.
- C. Any stacking lane greater than 100' in length shall provide for an escape lane.
- D. The design and construction of the drive-through facilities shall minimize the number of driveway cuts.

8-2C-44 Vehicle Washing Facility

A. Access and Parking. Safe pedestrian and vehicle access and circulation on the site and between adjacent properties shall be demonstrated as follows:

5. The design and construction of the drive-through facilities shall minimize the number of driveway cuts

8-4D-3B Parking Design and improvement Standards

2. Drainage: All parking and loading areas shall provide proper drainage of surface water to prevent the flow of water onto adjacent properties or walkways.

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8-4C-3 Non Residential Design D. Objective #4.

1. Maintenance of Existing Vegetation: All existing trees, significant vegetation and surface water features should be maintained.

8-4E-3B Transportation and Connectivity Provisions

Street Access:

3. Driveway access shall be separated from other driveways and street intersections in accordance with the requirements of the respective Transportation Authority, or as agreed to by the City and the Transportation Authority. Properties that do not conform to the requirements shall be brought into conformance when (a) the property is re-developed; (b) the property is altered by any structural addition that adds more than twenty-five percent (25%) increase in gross floor or (c) the property is used for a higher intensity use.

4. Access management controls, such as shared access, and/or access in variance with that specified by the Transportation Authority may be recommended by the City for the purpose of protecting the function, safety and functionality of the street.

5. Properties adjacent to Chinden Boulevard with alley access, shall provide access to the property from the alley.

Internal Vehicular Circulation

E. Driveways, aisles and turn-around areas, when required for fire and refuse access, shall meet the following standards:

1. Have a minimum vertical clearance of thirteen feet (13') six inches (6") for their entire length and width.

2. Have a minimum width of twenty feet (20').

3. The design of internal circulation should be integrated with the overall site design and adjacent properties, including the location of structures, pedestrian walkways and landscaping.

8-4I Landscaping and Tree Protection Provisions

3B. Retain and protect existing trees, vegetation, and native soils and integrate these features into the overall landscape plan.

4B. Landscaping for Townhouse, Two-Family Duplex, and Multi-Family Dwelling Units; Manufactured and Mobile Homes and All Non-Residential Uses.

1. A minimum of five percent (5%) of the gross site area shall be landscaped areas, excluding areas for setback or perimeter landscaping.

2. A minimum of one (1) Class III or Class II tree shall be planted in the frontage and every adjacent street side. An additional Class I tree shall be planted in the corresponding setback for every increment of fifty (50) feet linear feet of frontage.

a. Trees shall be selected from the from the City's approved list of appropriate tree species contained in the "Garden City Design and Construction Guide".

b. Trees shall be distributed throughout the site.

c. Shrubs shall be grouped and distributed throughout the site.

3. Trees may be substituted for up to one-half (1/2) of the required shrubs at the rate of one (1) tree for ten (10) shrubs and vice versa.

4. Class II or Class III trees can be substituted at the rate of two (2) Class I trees for every one (1) Class II or Class III trees unless otherwise specified by an adopted street design or master plan.

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8-4I-7 Tree Preservation Provisions

A. **Purpose.** The provisions of this section are intended to preserve existing trees of four inch (4") caliper or greater from destruction before and during the development process.

B. **Applicability.** Tree preservation is required in all districts.

C. **Standards.**

1. Site development shall make all feasible attempts to maintain existing trees four inch (4") caliper or greater within their design.

5. Mitigation shall be required for all existing trees four inch (4") caliper or greater that are removed or damaged from the site.

a. Mitigation is required for all such trees removed within eighteen (18) months prior to issuance of the building permit for construction on the site or damaged during construction.

b. Mitigation shall be replacement of the total calipers lost on site up to an amount of 100% replacement. [Example: two (2) 10-inch caliper trees removed may be mitigated with four (4) 5-inch caliper trees, five (5) 4-inch caliper trees, or seven (7) 3-inch caliper trees.]

c. Mitigation trees are in addition to all other landscaping required by this Article.

d. No mitigation is required in the following: (a) existing prohibited trees as specified in the "Garden City Design and Construction Guide" within the street buffer or parking lot; (b) existing dead, dying, or hazard trees certified prior to removal by a certified arborist; (c) trees that are required to be removed by another governmental agency having jurisdiction over the project.

Attachments:

location map

existing conditions pictures

site plans

elevations



3 VIEW FROM EAST
SCALE: 1/8" = 1'-0"



2 VIEW FROM SOUTH
SCALE: 1/8" = 1'-0"



1 VIEW FROM WEST
SCALE: 1/8" = 1'-0"

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revisions:

J'S ULTIMATE HAND CAR WASH
GARDEN CITY, IDAHO
3756 E. CHINDEN BOULEVARD
GARDEN CITY, IDAHO

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drawn	JEH
job no.	1506-01
issued for	PRELIM.

sheet title:
PERSPECTIVE ELEVATIONS

sheet number:
A2.1

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Application Questions

How does the design of the structure advance an urban form through its relationship to the street, the pedestrian and adjacent properties?

The structure increases the build to lot-area ratio. The structure is extended towards Chinden. Although there is no reason for pedestrian access with a car wash, there is a new customer service entrance at the control office that faces Chinden Blvd. The tilted canopy structure is inviting and activates the building from Chinden Blvd.

How does the design maximize the opportunities for safe and comfortable pedestrian accessibility and minimize the effects of parking and vehicular circulation?

Although the car wash has zero need for pedestrian access from the street, a sidewalk has been added on the Chinden Blvd. side of the building for the full-length of the building for safe customer access.

What are the building materials?

As noted on the perspective building elevations, the materials are metal wall panels, standing seam metal roof and metal fascia, stucco and aluminum entrances and storefront.

What are the existing notable site features and how does the design respect them?

Although nothing comes to mind as especially “notable”, the addition preserves most of the existing building aesthetic and adds additional interest with the stepping roof form and stucco volume. Where trees are removed, they are replaced. The landscape design proposed is extended to the adjacent properties along Chinden Blvd. as illustrated on the landscape plan.

Is the building consistent with the adopted streetscape?

This area of Chinden is somewhat dismal in terms of streetscape. J’s car wash has and will continue to improve the streetscape in this area.

Bike and Pedestrian: How have bike and pedestrian circulation been arranged with respect to adjacent facilities, internal circulation, and potential vehicular conflicts? Is there sidewalk? How far away are the nearest transit facilities and is there safe and comfortable access to the facilities?

The onsite bike and pedestrian circulation has been improved by the addition of the sidewalk adjacent to the building at the entire Chinden Blvd. side of the building. A bike rack is added in the portion of the existing building at that side that steps inward providing ample space for bikes and other pedestrians. Pedestrian and bike access to the adjacent properties is un-changed.

Parking and parking lot standards: Is there a tree provided for every 5 parking stalls? Is there bike parking provided? Is the parking adequately screened from adjacent uses and the street? Is there any stall that is located more than 100’ from a shade tree?

The service parking area at the backside of the building remains largely un-changed. We are not proposing additional trees at that concealed back-side of the building. Instead we are concentrating on the Chinden Blvd. side of the building and also the adjacent lots as well as a large shade tree at the southeast side of the building. There are not stalls located more than 100’ from the southeast shade tree.

Community Interaction: How does the development incorporate into the envisioned neighborhood? How does the proposed project support a compact development pattern that enables intensification of development and changes over time? How does the proposed design support a development pattern in nodes rather than strip commercial along arterial corridors? How does the project promote a place where people want to be? If not exempt 8-4G sustainability, how many points will the project have, as totaled from the sustainability checklist?

The development goes a long way to incorporate the notion of Chinden as a tree-line boulevard. The addition delivers a larger building on the same lot which is an obvious intensification of development. The design incorporates a new public entrance that faces Chinden which could represent the needs of a different business type in the future.

The full-block cohesive landscape plan development addresses not only the car wash but the neighbor properties very effectively as a node-style development rather than individual parcel development. The building and landscape aesthetic promotes a sense of place and where people will enjoy spending time.

This project is not subject to 8-4G compliance because the addition is less than 5,001 SF.

Landscaping: Is there more than 5% of the site dedicated to landscaping? Is there one class II or III tree provided for every 50' of street frontage? Will any trees be removed from the site? What kind of irrigation will be provided? Is the landscaping compatible with local climatic conditions?

Yes, more than 5% of the site is dedicated to landscaping. Yes, at least one class II or III tree is provided for every 50' of street frontage. There are several trees that will be removed and replaced along Chinden. Pressurized irrigation will be provided for all landscape areas. All vegetation selected is compatible with local climatic conditions and shall be maintained with the same vigilance that passersby have observed at the J's Car Wash facility for many, many years.

Building Design: How does the building provide visual interest and positively contribute to the overall urban fabric of the community? What is the Floor to Area ratio? Is there relief incorporated into facades and or rooflines greater than 50'? What are the setbacks? How are the outdoor service and equipment areas screened? If there are multiple structures, are the setbacks consistent? Are there any "green building" concepts are incorporated into the project?

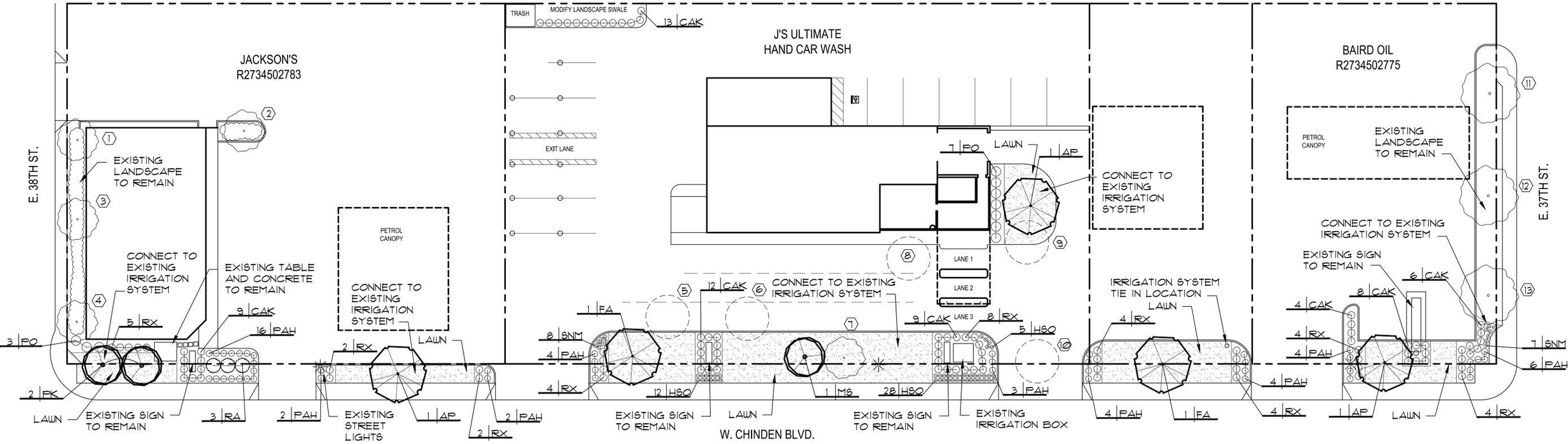
The building as it stands is very interesting and award-winning. The addition strives to complement and enhance the design as situated in the overall urban fabric of the community. The floor to area ratio including a mezzanine over the loading and equipment room addition is 0.19.

The design incorporates abundant relief with the walls and the roof of the structures. The setbacks are 5' at the side and rear yards and 14' from the property boundary at Chinden. These are illustrated on the site plan. Trash is contained inside of a masonry enclosure 6' tall. Other pre-existing equipment is screened with existing walls. We have incorporated canopies that will aim to shade some portions of the building. Low U value glazing and wall and roof insulation will aim to support "green building" concepts.



revisions:

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LANDSCAPE PLAN
SCALE: 1" = 20'-0"
NORTH



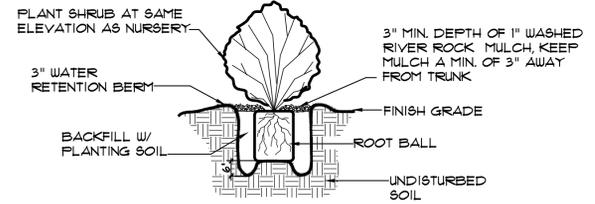
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PLANTING NOTES

- All plant material shall conform to the American Nurseryman Standards for type and size shown. Plants will be rejected if not in a sound and healthy condition.
- All lawn areas shall be sodded with 100% Dwarf Turf Type Tall Fescue.
- All sodded areas shall have 6" of topsoil and all planting beds shall have 18" of topsoil.
- All planting beds and tree wells in lawn areas (wells to be 3' in diameter) shall be covered with a minimum of 3" of small (1") Washed River Rock Mulch. Submit sample for approval by owner.
- All plant material shall be guaranteed for a period of one year beginning at the date of acceptance by Owner. Replace all plant material found dead or not in a healthy condition immediately with the same size and species at no cost to the Owner.
- Planting back fill for trees and planting beds shall be 5 parts topsoil and 1 part compost. Stake all trees per details.
- Topsoil shall be a loose, friable, sandy loam, clean and free of toxic materials, noxious weeds, weed seeds, rocks, grass, or other foreign material larger than 1" in any dimension, a pH from 5.5 to 7.0. Top soil from site shall be used, if meeting these standards. Place 1/2" compost over all planter areas and rototill into top 4". Spread, compact, and fine grade topsoil to a smooth and uniform grade, 1" below surface of walks and curbs in areas to be sodded and 3" in planting bed areas.
- Fertilize all trees and shrubs with 'Agriform' planting tablets, 21 gram. Quantity per manufacturer's recommendation.
- Plant material shall not be substituted without the written permission of Owner. Submit names of three suppliers contacted if substitution is requested and plant material specified is not available.
- Immediately clean up any topsoil, or other debris on site created from landscape operation and dispose of properly off site.
- All landscaped areas shall be irrigated with an automatic underground sprinkler system.

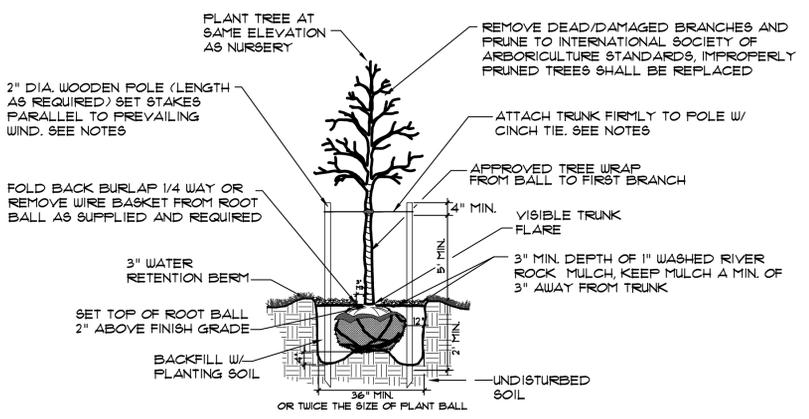
EXISTING TREES

- ① 8" Flowering Pear to remain
- ② 6" Flowering Pear to remain
- ③ 8" Flowering Pear to remain
- ④ 10" Flowering Pear to remain
- ⑤ 8" Crab to be removed
- ⑥ 12" Crab to be removed
- ⑦ 12" Crab to remain
- ⑧ 8" Columnar Maple to be removed
- ⑨ 8" Columnar Maple to be removed
- ⑩ 8" Crab to be removed
- ⑪ 10" Maple to remain
- ⑫ 10" Maple to remain
- ⑬ 10" Maple to remain



NOTE - DIG HOLE TWICE THE SIZE OF ROOT BALL

① SHRUB PLANTING DETAIL
NO SCALE



- NOTES:**
- THE STAKING OF TREES IS TO BE THE CONTRACTOR'S OPTION HOWEVER, THE CONTRACTOR IS RESPONSIBLE TO INSURE THAT ALL TREES ARE PLANTED STRAIGHT AND THAT THEY REMAIN STRAIGHT FOR THE TERM OF THE WARRANTY.
 - WRAP RUBBER CINCH TIE (OR APPROVED EQUAL) AROUND THE TREE TRUNKS AND STAKES USING EITHER THE STANDARD OR FIGURE EIGHT METHOD. SECURE THE TIES TO THE STAKES WITH GALVANIZED NAILS TO PREVENT MOVEMENT.
 - WATER IN BACKFILL BELOW THE ROOT BALL PRIOR TO SETTING TREE (TO INSURE NO SETTLING WILL OCCUR).
 - DEEP SOAK TREE TWICE WITHIN THE FIRST 48 HOURS.

② DECIDUOUS TREE PLANTING DETAIL
NO SCALE

PLANT SCHEDULE

KEY	BOTANICAL NAME COMMON NAME	PLANTING SIZE
TREES		
AP	Acer platanoides 'Columnarobroad' Parkway Maple	3" B4B
FA	Fraxinus americana 'Junginger' Autumn Purple Ash	3" B4B
MS	Malus 'Spring Snow' Spring Snow Crab	3" B4B
FK	Prunus cerasifera 'Krauter Vesuvius' Krauter Vesuvius Plum	3" B4B
SHRUBS		
FO	Physocarpus opulifolius 'Summerwine' Summerwine Ninebark	5 Gal.
RA	Rhus aromatica 'Grow Low' Grow Low Sumac	5 Gal.
RX	Rosa x 'Chateau Merlot' Merlot Flowering Carpet Rose	2 Gal.
GROUND COVERS/ PERENNIALS/GRASSES		
CAK	Calamagrostis x acutiflora 'Karl Forester' Karl Forester Grass	1 Gal.
H8O	Hemerocallis x 'Stella de Oro' Stella de Oro Daylily	1 Gal.
PAH	Festisetum alopecuroides 'Hamsin' Dwarf Fountain Grass	1 Gal.
SNM	Salvia nemorosa 'May Night' May Night Salvia	1 Gal.

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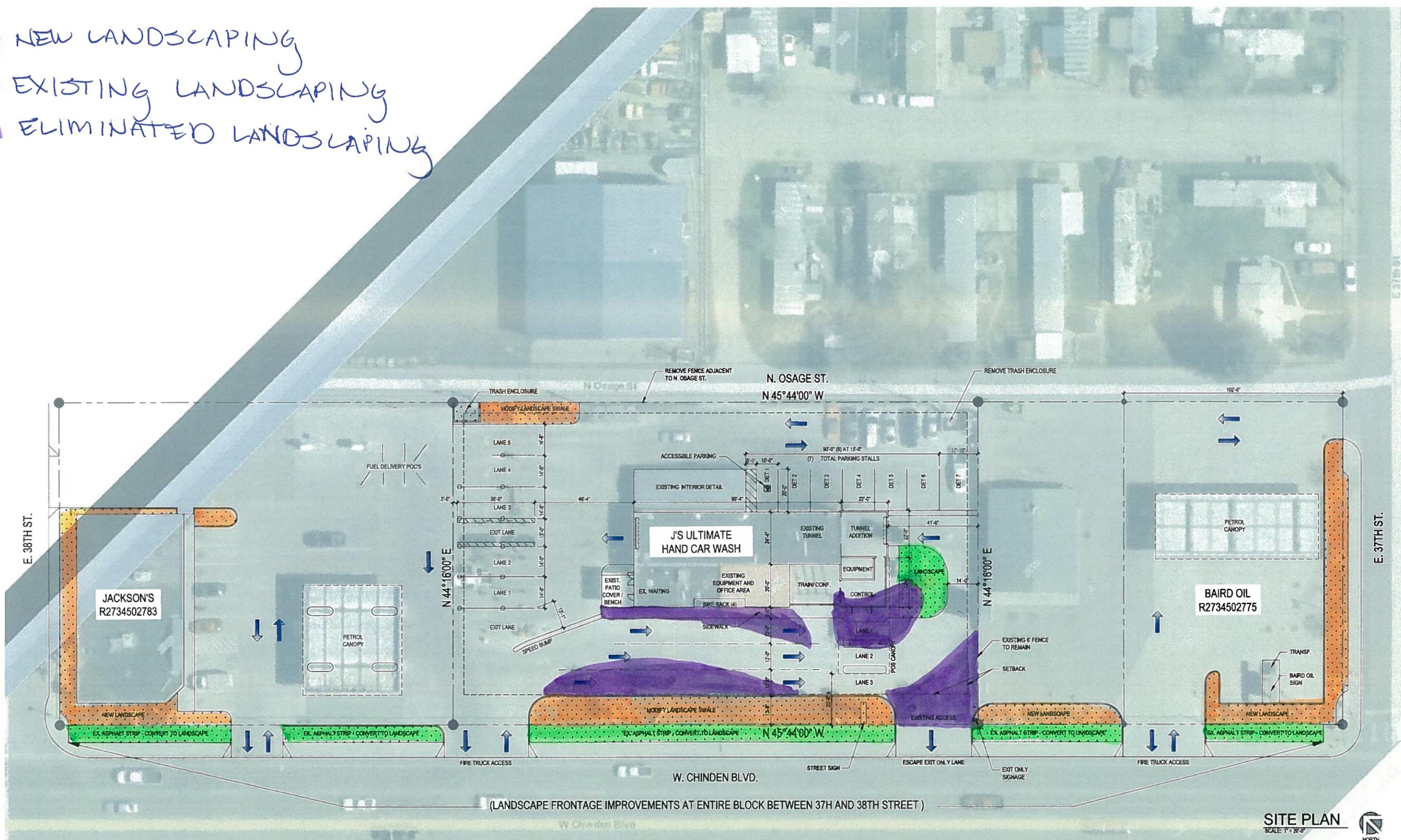
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LANDSCAPE PLAN

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- NEW LANDSCAPING
- EXISTING LANDSCAPING
- ELIMINATED LANDSCAPING



(LANDSCAPE FRONTAGE IMPROVEMENTS AT ENTIRE BLOCK BETWEEN 37TH AND 38TH STREET)

SITE PLAN
SCALE: 1" = 20'-0"

PARCEL: R2734502783
OWNER: JACKSON
ZONE CODE: C-1
SIZE OF LOT: 0.627 ACRES / 27,312 SF
C-STORE: 3,582 SF
EXISTING PETROL CANOPY: 2,346 ± SF

PROPERTY DESCRIPTION:
LOT 34 & NW 85' OF LOT 33 BLK 9 EXC RW FAIRVIEW ACRES
SUB 1 #2782-B #88006315

PARCEL: R2734502777
OWNER: CHINDEN LLC
ZONE CODE: C-1
SIZE OF LOT: 0.841 ACRES / 36,634 SF
EXISTING BUILDING: 3,790 SF
NEW TUNNEL ADDITION: 1,510 SF
NEW CONTROL OFFICE: 280 SF
NEW POS CANOPY: 872 SF

PROPERTY DESCRIPTION:
LOT 03 & LOT 32 & POR OF LOT 02 & LOT 33 BLK 09 FAIRVIEW
ACRES SUB NO 01 PARCELA R/S 6045 #2776-C #2774-S

PARCEL: R2734502775
OWNER: BAIRD
ZONE CODE: C-1
SIZE OF LOT: 0.59 ACRES / 25,700 SF
SMOKE SHOP: 1,760 SF
EXISTING PETROL CANOPY: 2,280 ± SF

PROPERTY DESCRIPTION:
LOT 01 & POR OF LOT 02 BLK 09 FAIRVIEW ACRES SUB NO 01
PARCEL B R/S 6045 #2774-B

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SITE PLAN

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E 38th St

Osage St

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U.S. Hwy 26

Google earth

E 37th St



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Exterior
Hand Washes
\$19.99 PER
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TO
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BETWEEN 4000
& 5000
S.W. 100 THRU 700
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